

CITY OF GAINESVILLE
2020 MASTER THOROUGHFARE AND
SIDEWALK PLAN

MARCH 2021



Prepared by:

Kimley»Horn

Kimley-Horn and Associates, Inc.

TBPE No. 928
801 Cherry Street, Unit 11
Suite 1300
Fort Worth, TX 76132
(817) 335-6511



3/16/2021

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1.0 EXECUTIVE SUMMARY

This report presents the analysis, findings, and recommendations for the City to meet their sidewalk and roadway infrastructure needs over the next 25 years. The goal of the Master Thoroughfare and Sidewalk Plan was to develop a plan for Gainesville (City) to improve network connectivity, mobility, and safety for motorists and pedestrians with the City and its ETJ. The planning time frame for the Master Thoroughfare and Sidewalk plan was 25-years, through the year 2045.

2.0 SIDEWALK PLAN AND PROJECT PRIORITIZATION

Existing Inventory

To begin, a detailed look through Google Earth was used to determine sidewalk presence within the City limits. This was completed using data available in the summer of 2020. Each street was examined for the presence of sidewalk and this was documented in ArcGIS.

Finally, the locations of schools, parks, and trails were identified and documented. The final existing inventory is documented in **Exhibit A**.

Prioritization

Once the existing inventory was completed, the prioritization of sidewalk gaps began. A quarter-mile radius buffer was drawn around each park and school and a half-mile radius buffer was drawn around the Home Grown Hero Trail. These distances are based on commonly used planning practices, and generally reflect people's walking tendencies. Based on discussions with City staff, one of the highest priority routes for pedestrian connectivity is the route between Gainesville High School and North Central Texas College to the downtown area along California Street. This route is shown on the priority map as a "missing link."

Given these parameters, prioritization was assigned. High prioritization was assigned where any of the radii/buffers overlapped with another. Medium prioritization was assigned to the radii/buffer area immediately outside of the high priority areas. Low prioritization was assigned to all radii/buffer areas that did not overlap with others.

Exhibit B shows the resulting sidewalk prioritization areas.

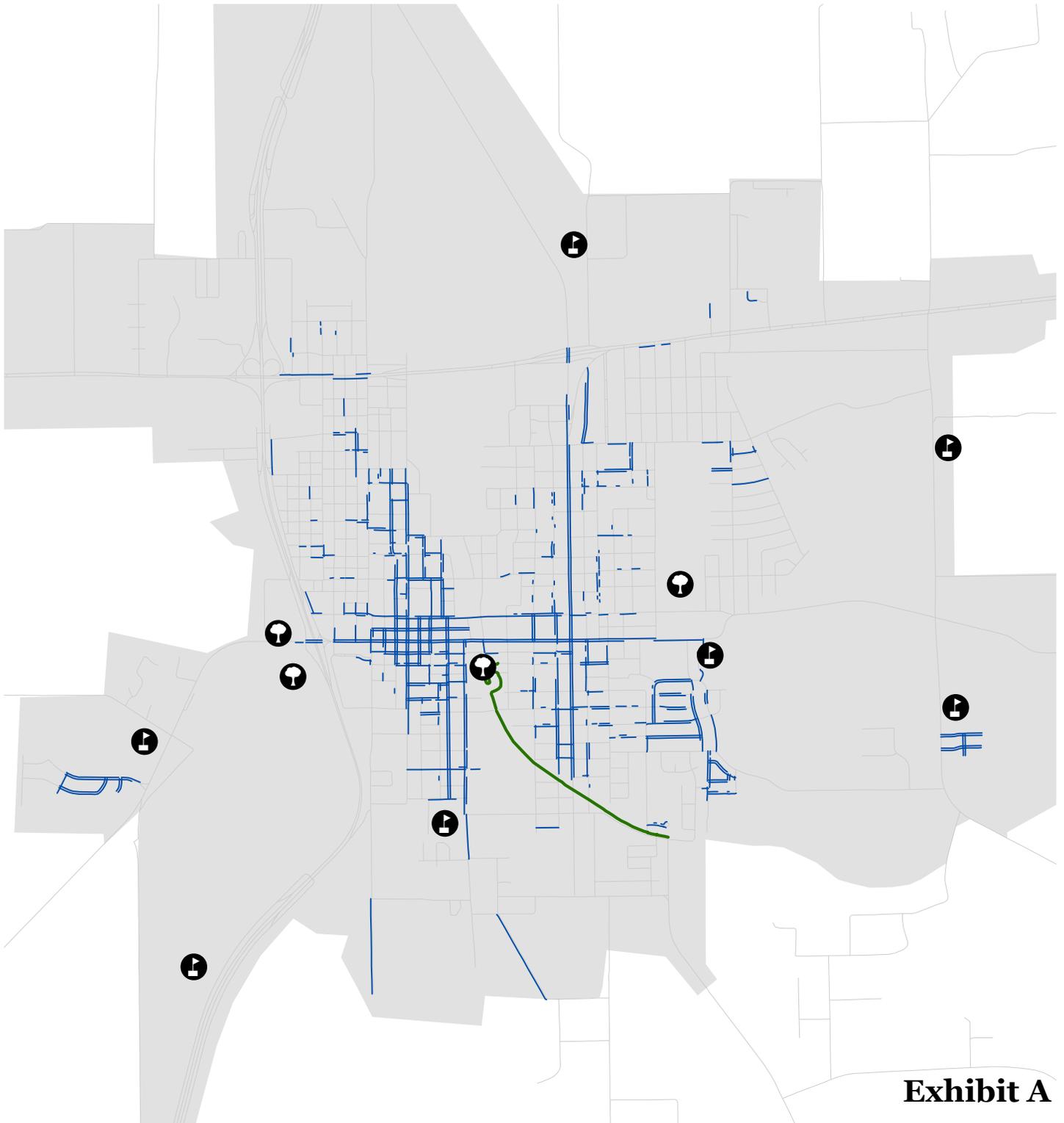


Exhibit A

	<h2>SIDEWALK INVENTORY</h2> <p><i>City of Gainesville, TX</i></p>													
	<p>0 0.25 0.5 Miles</p> 	<table border="0"> <tr> <td></td> <td>Roads</td> <td></td> <td>Parks</td> </tr> <tr> <td></td> <td>Sidewalk</td> <td></td> <td>Schools</td> </tr> <tr> <td></td> <td>Trail</td> <td></td> <td></td> </tr> </table>		Roads		Parks		Sidewalk		Schools		Trail		
	Roads		Parks											
	Sidewalk		Schools											
	Trail													

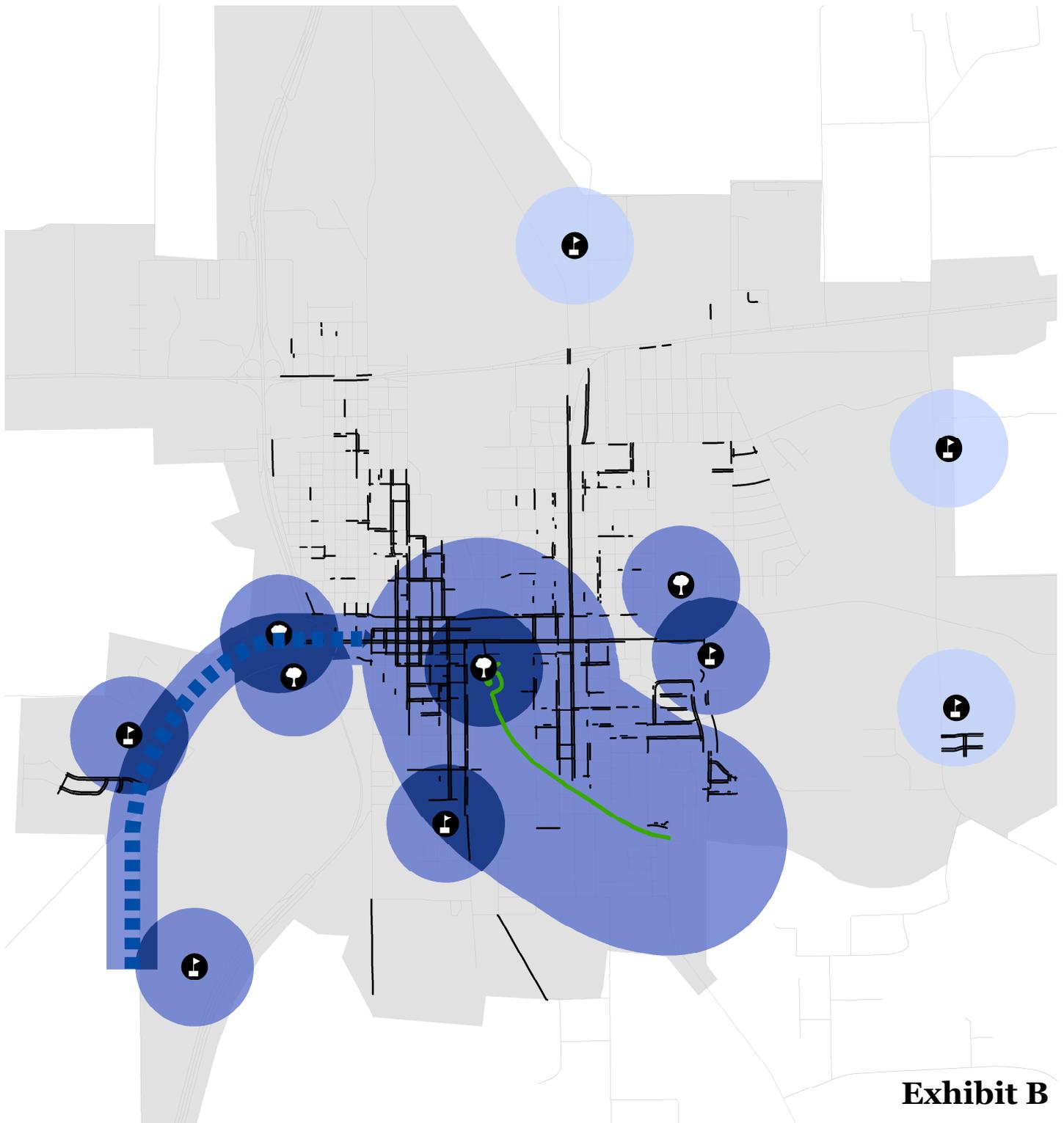


Exhibit B





0

0.25

0.5

Miles

SIDEWALK PRIORITY AREAS

City of Gainesville, TX

Sidewalks	Parks	High Priority
Trail	Schools	Medium Priority
Roads		Low Priority
		Missing Link

2.1 SIDEWALK CIP PROJECTS

Based on the prioritization process, several projects were identified for recommendation to include in the Sidewalk Capital Improvement Plan. **Exhibits C-G** identify each of the project locations, as well as their relation to existing sidewalks and trails.

West California Street

West California Street is recommended to have a continuous six-foot sidewalk on each side of the street from Gainesville High School to Culberson Street (10,682 feet). This segment would provide a pedestrian connection from Gainesville High School, North Central Texas College, and Moffett and Leonard Parks, across I-35 to downtown Gainesville. Some segments of five-foot sidewalk currently exist along California. It is recommended to replace all five-foot sidewalk with a six-foot sidewalk; where replacement would cause the project to be financially infeasible, the City may consider keeping the five-foot width where it currently exists until redevelopment would allow replacement.

These proposed sidewalks would collectively cross 30 driveways that may need to be reconstructed and would require approximately 600' of a retaining wall on both sides of the roadway. There is an existing bridge that has limited right-of-way, but there is 5' of existing sidewalk on each side. The approximate cost for this project is \$1,461,337.

West California Street – Priority Route

An alternative to the West California Street project would be to build the sidewalk only on the east side of FM 1306 and the west side of West California Street, including the conversion of the flashing traffic signal at FM 1306 and California Street to a fully signalized intersection with a pedestrian crossing. This would allow direct pedestrian access between the High School and College and the Downtown area. To access amenities on the south side of the roadway east of the bridge (Leonard Park, Frank Buck Zoo, and the Gainesville War Memorial), an existing signalized crossing is provided at the southbound I-35 frontage road intersection with West California Street.

This proposed sidewalk would cross 16 driveways that may need to be reconstructed and would require approximately 600' of a retaining wall on the south side of the roadway. There is an existing bridge that has limited right-of-way, but there is 5' of existing sidewalk provided. The approximate cost for this project is \$740,794.

East Main Street

East Main Street is recommended to have a continuous six-foot sidewalk on each side of the street from Red River Street to South Grand Avenue (3,139 feet). There are limited east-west connections across the existing railroad and waterway. East Main Street is one of the few existing roadways that does traverse these barriers. It also provides an alternative to the higher classification roadway of

California Street as well as a direct connection to the Home Grown Hero Trail. Sidewalk currently exists along portions of the project area. If the existing sidewalk within the project area is less than five feet in width, it is recommended to replace the existing sidewalk with the wider five-foot width.

These proposed sidewalks would collectively cross 36 driveways that may need to be reconstructed. No retaining walls are anticipated. There are several obstructions in the right-of-way that may need to be addressed at time of construction, including: 37 poles providing overhead utilities, other poles that obstruct the path of the existing sidewalk, constrained right-of-way along some portions, a railroad crossing, and an existing bridge. The bridge has 6' of existing sidewalk along the south side of the roadway. The approximate cost for this project is \$479,714.

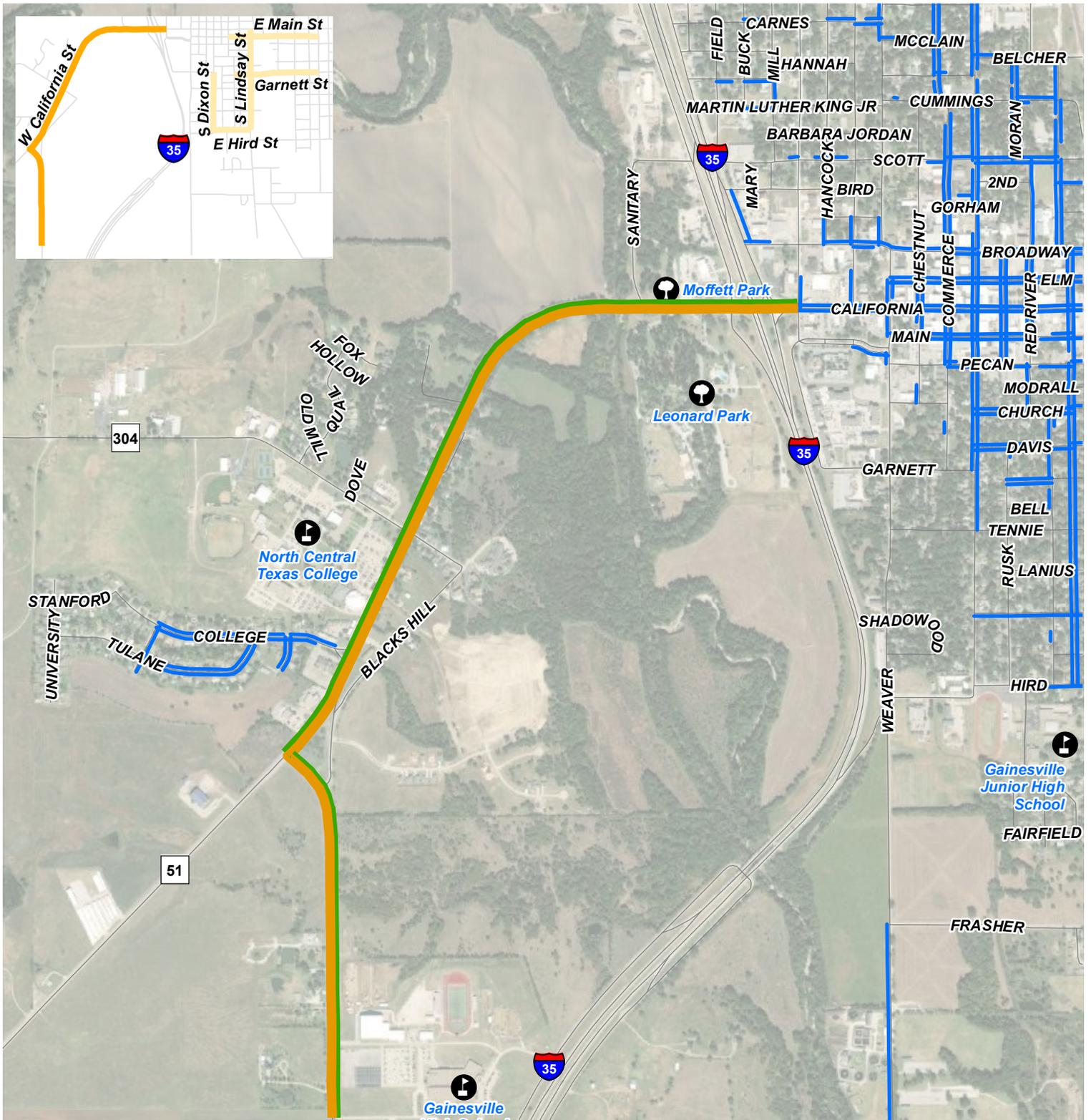


Project Details

Roadway: W California St
Limits: Gainesville High School to Culberson St
Length: 10,682 feet
Cost: \$ 1461337
Driveways: 30
Retaining Wall: Yes, 600'
Obstacles: Bridge - existing 5' sidewalk on both sides

Sidewalk Location		Existing Facilities	
	Both sides of roadway		Sidewalk
	Priority route		Trail

Gainesville
 0 625 1,250 Feet



Project Details

Roadway: W California St - Priority

Limits: Gainesville High School to Culberson St

Length: 10,682 feet

Cost: \$740,794

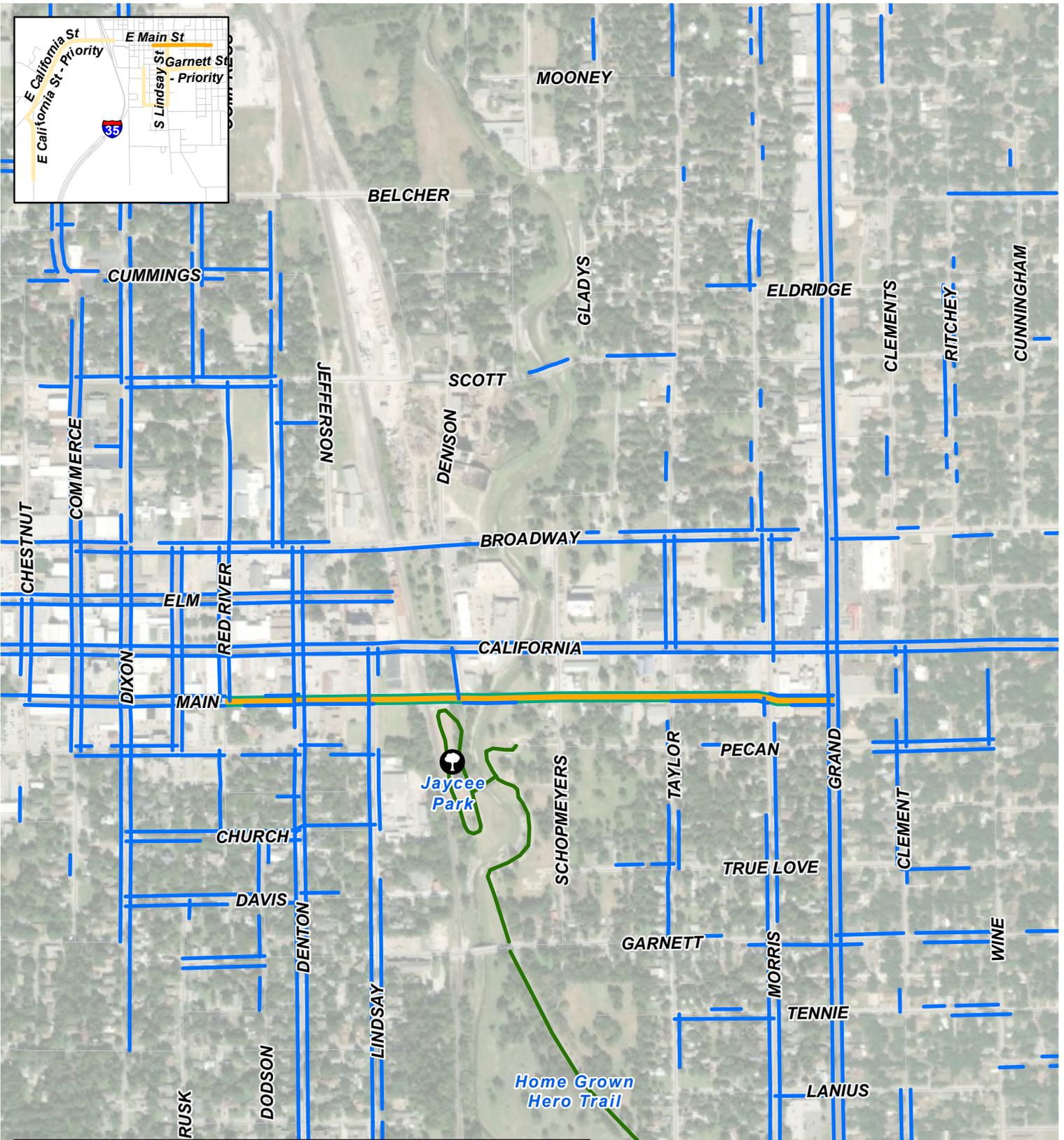
Driveways: 16

Retaining wall: Yes, 600'

Obstacles: Bridge - existing 5' sidewalk

Sidewalk Location	Existing Facilities
Both sides of roadway	Sidewalk
Priority route	Trail

Source: Esri, DigitalGlobe



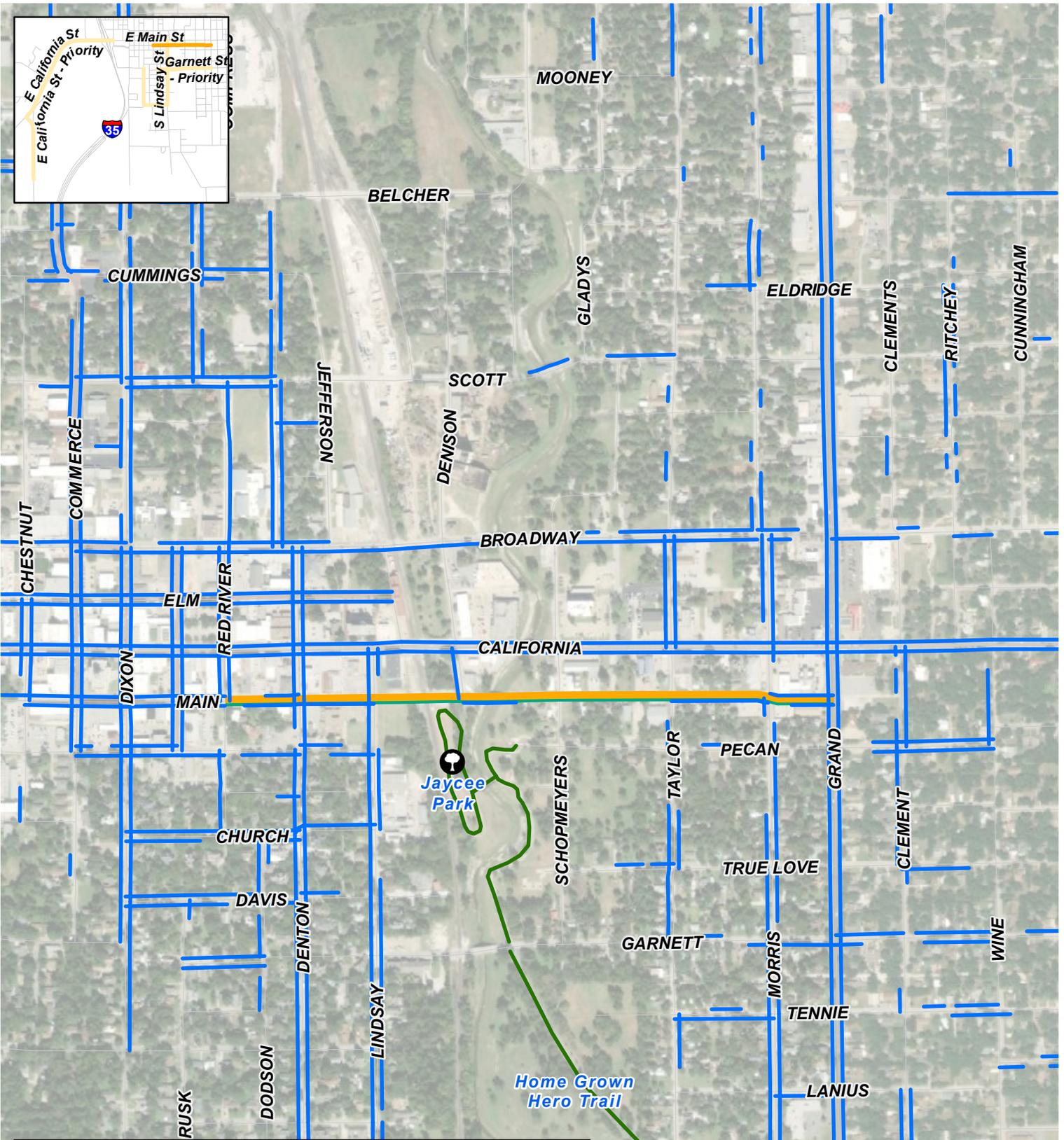
Project Details

Roadway: E Main St
Limits: Red River St to S Grand Ave
Length: 3,139 feet
Cost: \$ 479714
Driveways: 36
Retaining Wall: No
Obstacles: Poles for overhead lines - 37; constrained ROW, poles in existing sidewalk; rail crossing; bridge with existing 6' sidewalk on south side

Sidewalk Location		Existing Facilities	
	Both sides of roadway		Sidewalk
	Priority route		Trail

Gainesville
 Totally Texas. All American.

0 345 690 Feet



Project Details

Roadway: E Main St - Priority

Limits: Red River St to S Grand Ave

Length: 3,139 feet

Cost: \$ 241882

Driveways: 19

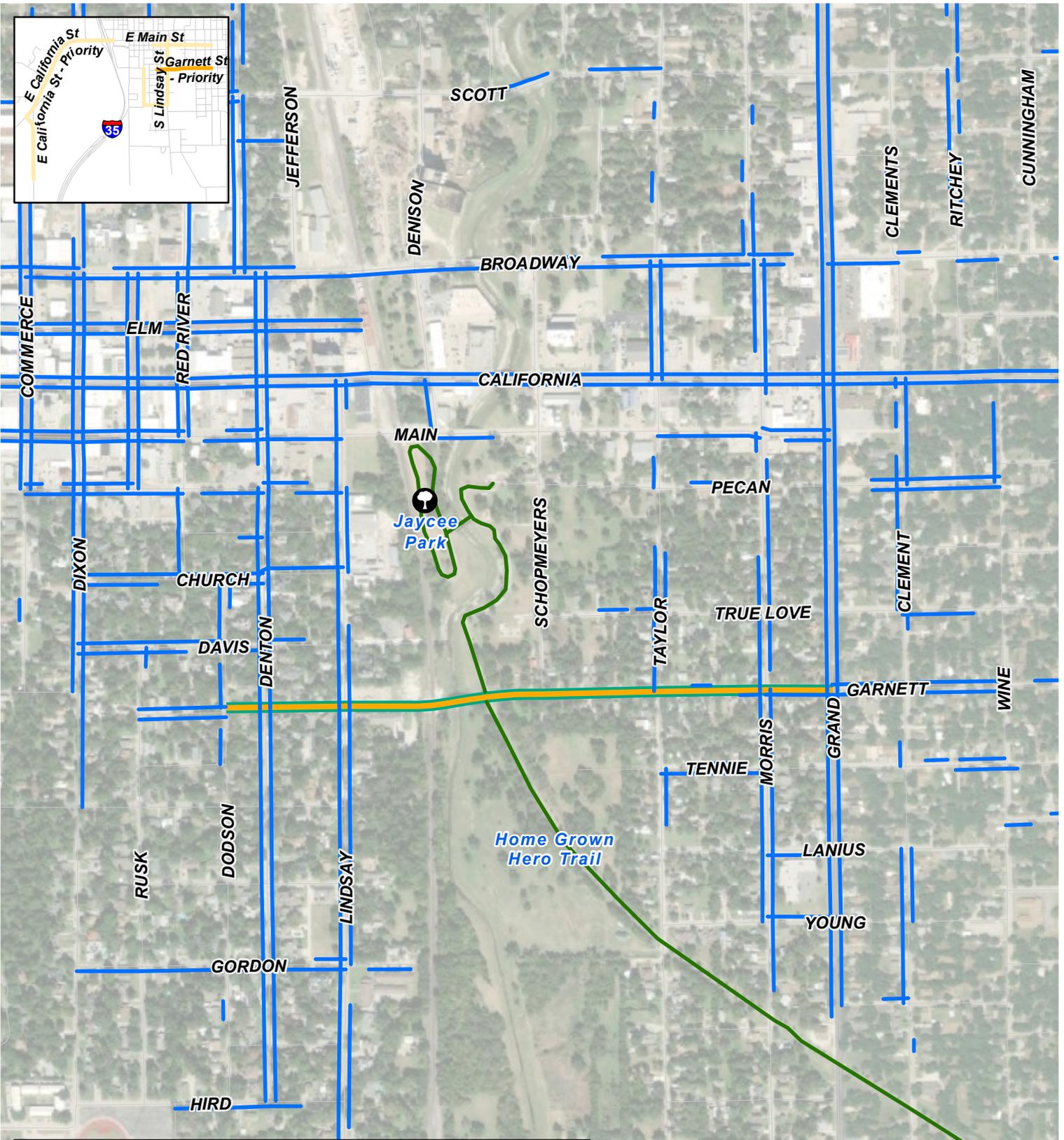
Retaining Wall: No

Obstacles: Poles for overhead lines - 17; constrained ROW, poles in existing sidewalk; rail crossing; bridge with existing 6' sidewalk

Sidewalk Location		Existing Facilities	
	Both sides of roadway		Sidewalk
	Priority route		Trail

Gainesville
Totally Texas. All American.

0 345 690 Feet



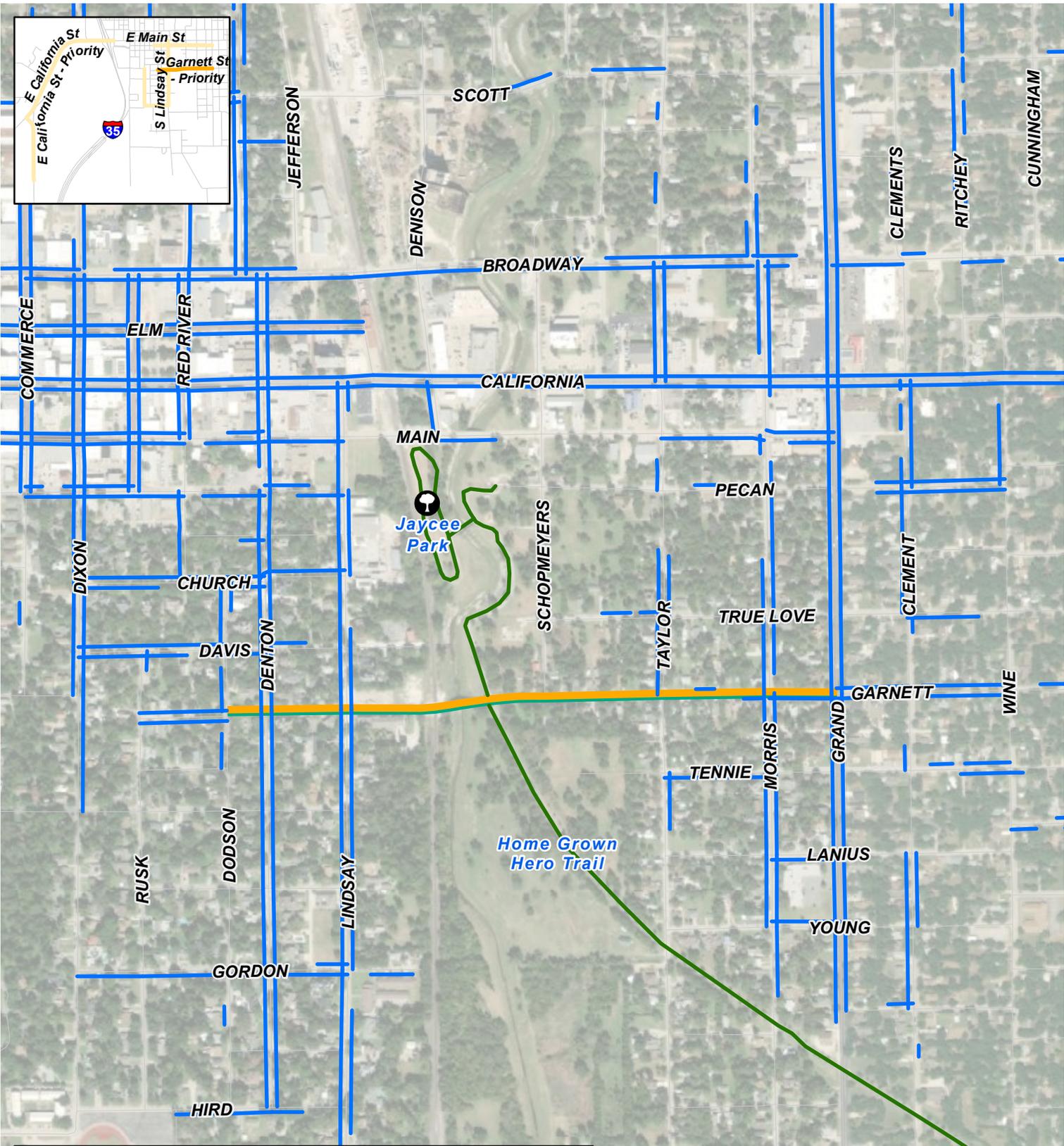
Project Details

Roadway: Garnett St
Limits: Dodson St to S Grand Ave
Length: 2,948 feet
Cost: \$ 463736
Driveways: 37
Retaining Wall: Yes, 250'
Obstacles: Poles for overhead lines - 27; constrained ROW, rail crossing; bridge with existing 6' sidewalk on south side

Sidewalk Location		Existing Facilities	
	Both sides of roadway		Sidewalk
	Priority route		Trail



0 325 650 Feet

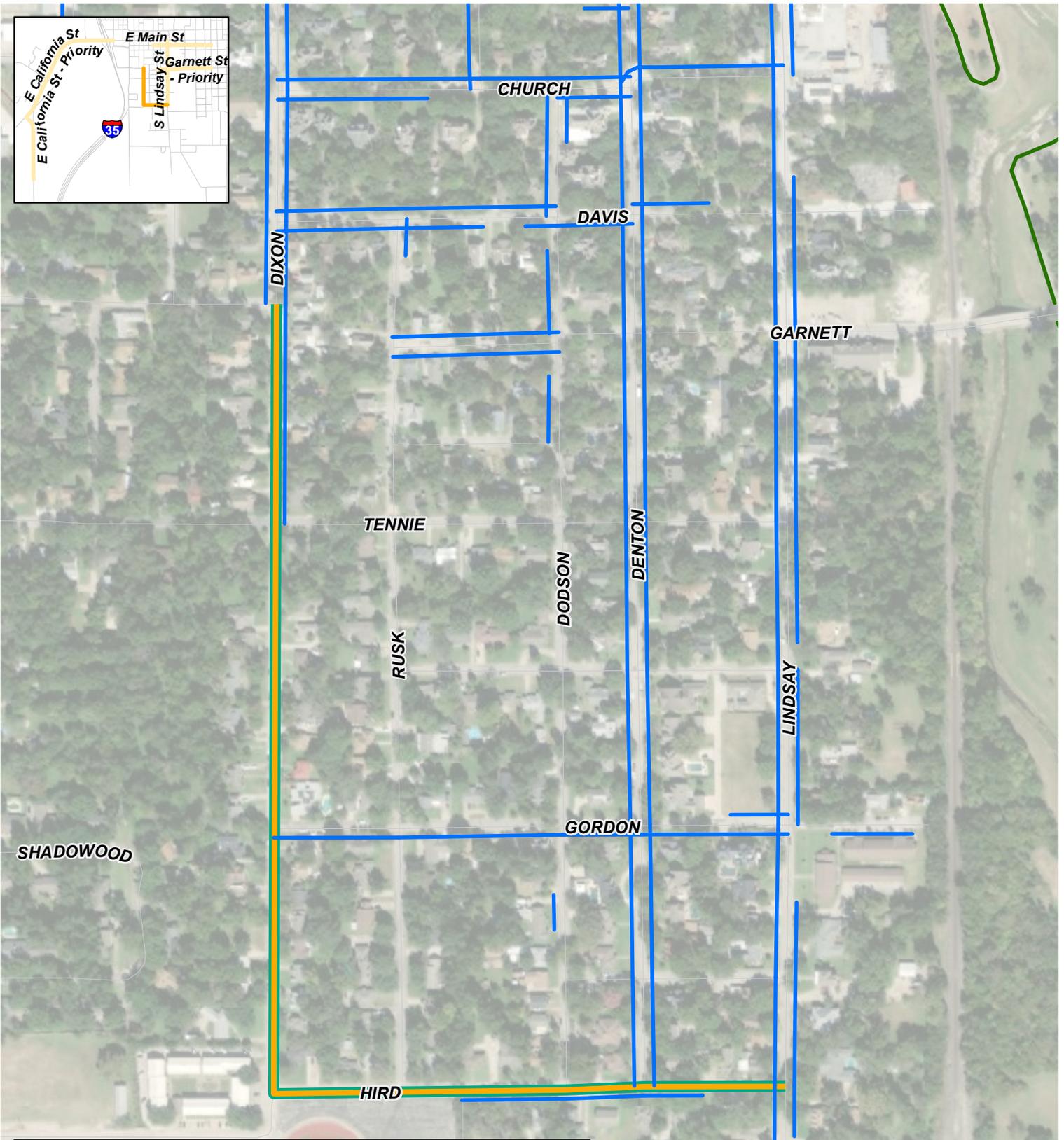
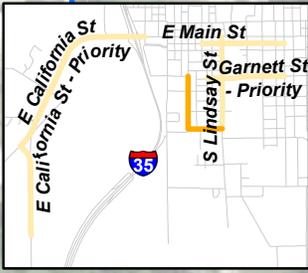


Project Details

Roadway: Garnett St - Priority
Limits: Dodson St to S Grand Ave
Length: 2,948 feet
Cost: \$ 242330
Driveways: 22
Retaining Wall: Yes, 250'
Obstacles: Poles for overhead lines - 14; constrained ROW, rail crossing; bridge with existing 6' sidewalk

Sidewalk Location		Existing Facilities	
	Both sides of roadway		Sidewalk
	Priority route		Trail

0 325 650 Feet



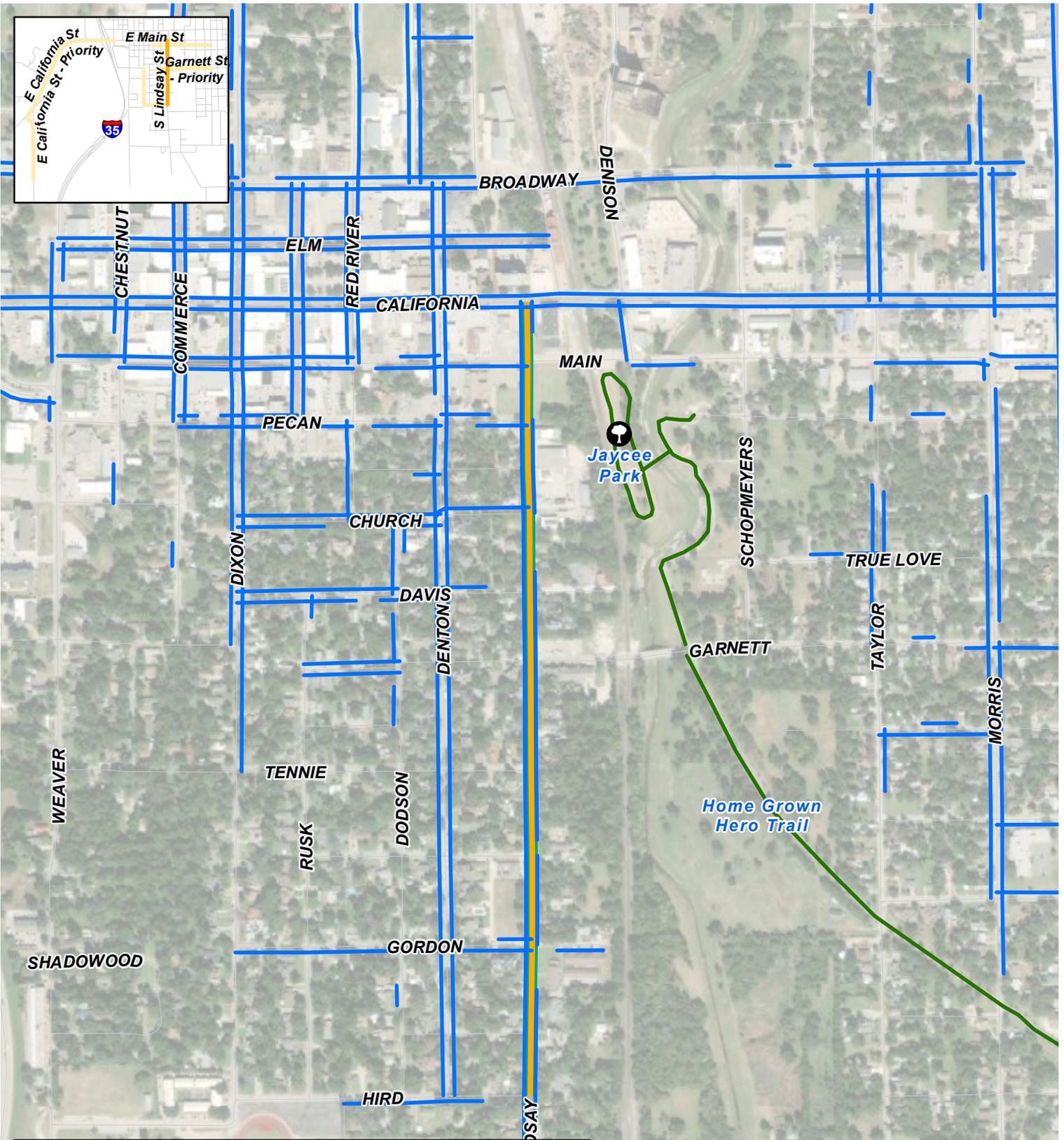
Project Details

Roadway: S Dixon St/E Hird St
Limits: Garnett St to S Lindsay St
Length: 3,309 feet
Cost: \$ 540221
Driveways: 55
Retaining Wall: No
Obstacles: Poles for overhead lines - 42

Sidewalk Location		Existing Facilities	
	Both sides of roadway		Sidewalk
	Priority route		Trail

Gainesville
 Totally Texas. All American.

0 170 340 Feet



Project Details	
Roadway:	S Lindsay St
Limits:	E California St to E Hird St
Length:	3,504 feet
Cost:	\$ 310084
Driveways:	41
Retaining Wall:	No
Obstacles:	Poles for overhead lines - 6

Sidewalk Location	Existing Facilities
Both sides of roadway	Sidewalk
Priority route	Trail

East Main Street – Priority Route

An alternative to the East Main Street project would be to build the sidewalk only on the south side of East Main Street. This would allow for continuous sidewalk along one side of the roadway, including access across the existing bridge. The bridge provides 6' of existing sidewalk.

This proposed sidewalk would cross 19 driveways that may need to be reconstructed. No retaining walls are anticipated. There are several obstructions in the right-of-way that may need to be addressed at time of construction, including: 17 poles providing overhead utilities, other poles that obstruct the path of the existing sidewalk, constrained right-of-way along some portions, a railroad crossing, and an existing bridge. As previously mentioned, the bridge provides 6' of existing sidewalk. The approximate cost for this project is \$241,882.

Garnett Street

Garnett Street is recommended to have a continuous six-foot sidewalk on each side of the street from Dodson Street to South Grand Avenue (2,948 feet). There are limited east-west connections across the existing railroad and waterway. Garnett Street is one of the few existing roadways that does traverse these barriers. It also provides a direct connection to the Home Grown Hero Trail. Sidewalk currently exists along portions of the project area. If the existing sidewalk within the project area is less than five feet in width, it is recommended to replace the existing sidewalk with the wider five-foot width.

These proposed sidewalks would collectively cross 37 driveways that may need to be reconstructed. A retaining wall would be required along approximately 250' of the segment, east of the bridge. There are several obstructions in the right-of-way that may need to be addressed at the time of construction, including: 27 poles providing overhead utilities, constrained right-of-way along some portions, a railroad crossing, and an existing bridge. The bridge has 6' of existing sidewalk along the south side of the roadway. The approximate cost for this project is \$463,736.

Garnett Street – Priority Route

An alternative to the Garnett Street project would be to build the sidewalk only on the south side of Garnett Street. This would allow for continuous sidewalk along one side of the roadway, including access across the existing bridge. The bridge provides 6' of existing sidewalk.

This proposed sidewalk would cross 22 driveways that may need to be reconstructed. A retaining wall would be required along approximately 250' of the segment, east of the bridge. There are several obstructions in the right-of-way that may need to be addressed at the time of construction, including: 14 poles providing overhead utilities, constrained right-of-way along some portions, a railroad crossing, and an existing bridge. As previously mentioned, the bridge has 6' of existing sidewalk along the south side of the roadway. The approximate cost for this project is \$242,330.

South Dixon Street/East Hird Street

South Dixon Street/East Hird Street is recommended to have a continuous six-foot sidewalk on each side of the street from Garnett Street to South Lindsay Street (3,309 feet). This connection would provide a pedestrian connection to and from Gainesville Junior High School and Leeper Stadium. It would also fill a sidewalk gap for the residential uses along South Dixon Street north to the downtown area. Sidewalk currently exists north of the project area along South Dixon Street and along a portion of the south side of East Hird Street within the project area. The new sidewalk should connect to these existing sidewalks. If the existing sidewalk within the project area is less than five feet in width, it is recommended to replace the existing sidewalk with the wider five-foot width.

These proposed sidewalks would collectively cross 55 driveways that may need to be reconstructed. No retaining walls are anticipated. There are 42 poles in the right-of-way that provide overhead utilities that may need to be addressed at the time of construction. The approximate cost for this project is \$540,221.

South Lindsay Street

South Lindsay Street is recommended to have a continuous six-foot sidewalk on each side of the street from East California Street to East Hird Street (3,504 feet). This connection would provide another north-south connection between Gainesville Junior High School and the adjacent homes to the downtown area. Existing sidewalk of varying widths currently exists along much of South Lindsay Street in the project area. The new sidewalk should connect to these existing sidewalks. If the existing sidewalk within the project area is less than five feet in width, it is recommended to replace the existing sidewalk with the wider five-foot width.

These proposed sidewalks would collectively cross 41 driveways that may need to be reconstructed. No retaining walls are anticipated. There are 6 poles in the right-of-way that provide overhead utilities that may need to be addressed at the time of construction. The approximate cost for this project is \$310,084.

3.0 THOROUGHFARE PLAN

To create the Thoroughfare Plan, the Cooke County Thoroughfare Plan was used as a base. As it is a County Thoroughfare Plan, regional connectivity was likely the primary focus. To make the City's Thoroughfare Plan more attuned to local needs, alignments from the Cooke County Thoroughfare Plan were realigned, deleted, or the thoroughfare classification was changed. Alignments were also added to provide more connectivity. More details about each of these categories of changes are provided below. All changes to the Cooke County Thoroughfare Plan are documented in **Exhibit H**. The resulting Thoroughfare Plan map is provided in **Exhibit I**.

3.1 ADDITIONAL ALIGNMENTS

Thoroughfare alignments were added to create a more comprehensive thoroughfare network that generally provides a grid with links approximately ½ mile – 1 mile apart. In the central City area, there were several barriers to providing continuous connections, including the railroad, waterways, and existing right-of-way constrictions. Therefore, the focus in the central city area was on providing connections across these barriers using existing roadways that would not require further right-of-way takings. As the City continues to develop, additional parcels could be purchased by the City to make smooth transitions and continuous street radii where needed.

For the entire study area, existing roadways were used wherever possible. Major barriers were also avoided in the recommendation of new alignments; these barriers included the Gainesville Municipal Airport, the floodplain and the railroad. There were no new crossings of the railroad proposed, with the exception of Anthony Street. The City has already acquired the necessary right-of-way to make a future connection along Anthony Street across the railroad and TxDOT has incorporated this crossing into its future planning efforts, through the Cross Plains Rural Transportation Council.

3.2 REALIGNMENTS/DELETIONS

There were only two proposed realignments/deletions of thoroughfares that are proposed on the Cooke County Thoroughfare Plan:

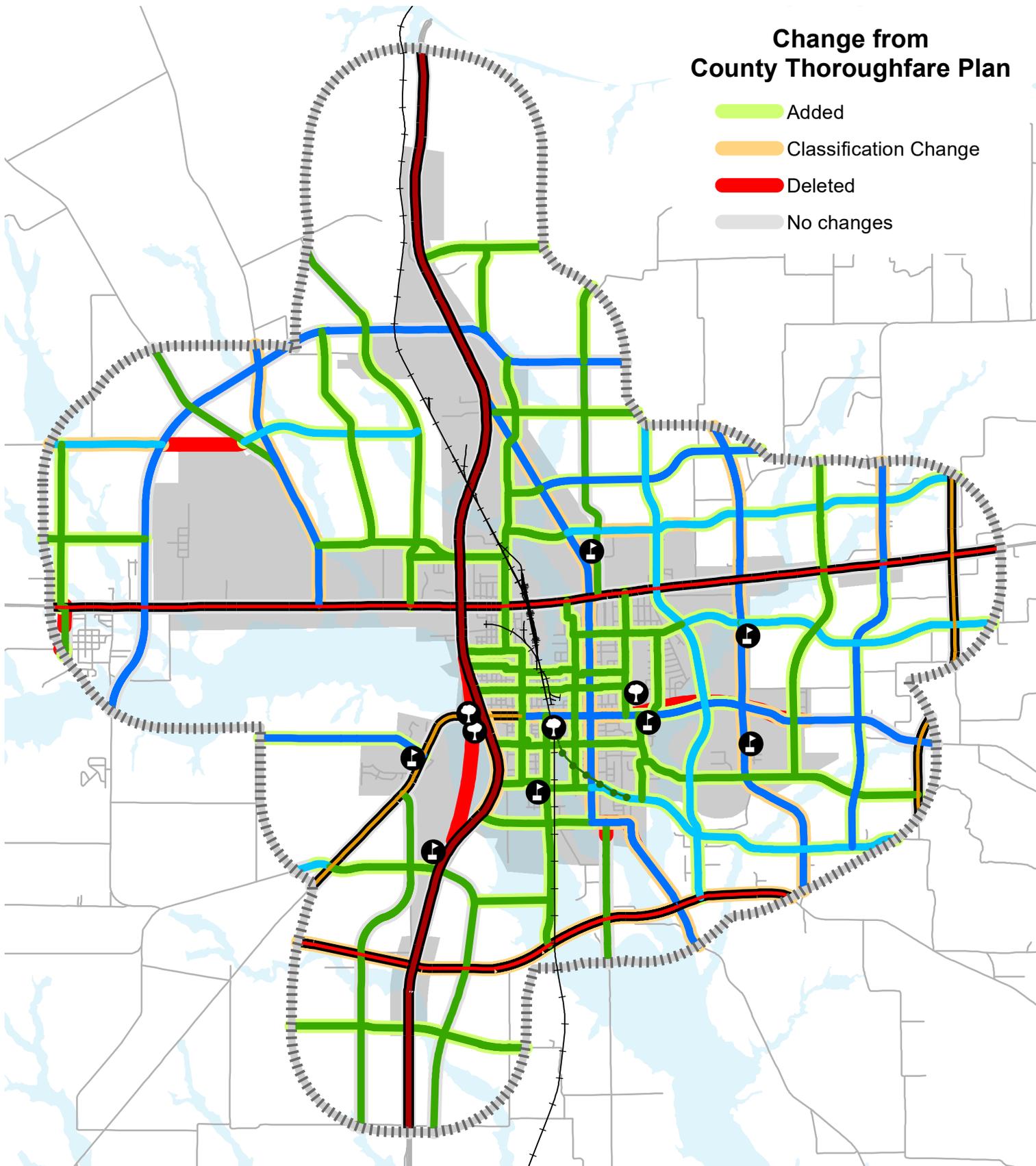
- The County's proposed alignment of I-35 was realigned to more closely match the current TxDOT EA alignment. This realignment avoids conflicts with Gainesville High School, the Gainesville War Memorial, Frank Buck Zoo, and Leonard and Moffitt Parks.
- The East California Street alignment, where it transitions to FM 678, was realigned to smooth the geometry of the curve. This will allow for more appropriate speeds on a higher functional class roadway, as vehicles will not need to slow down as much to navigate the curve.

3.3 CLASSIFICATION CHANGES

Classifications were changed from the proposed classifications on the Cooke County Thoroughfare Plan in multiple instances. Where a roadway was providing regional benefit, traversed a greater distance, or served a greater purpose for the City of Gainesville, the classification was upgraded.

Change from County Thoroughfare Plan

- █ Added
- █ Classification Change
- █ Deleted
- █ No changes



2020 THOROUGHFARE PLAN



Thoroughfare Plan

- █ Freeway/Highway
- █ Frontage
- █ Regional Arterial

Principal Arterial

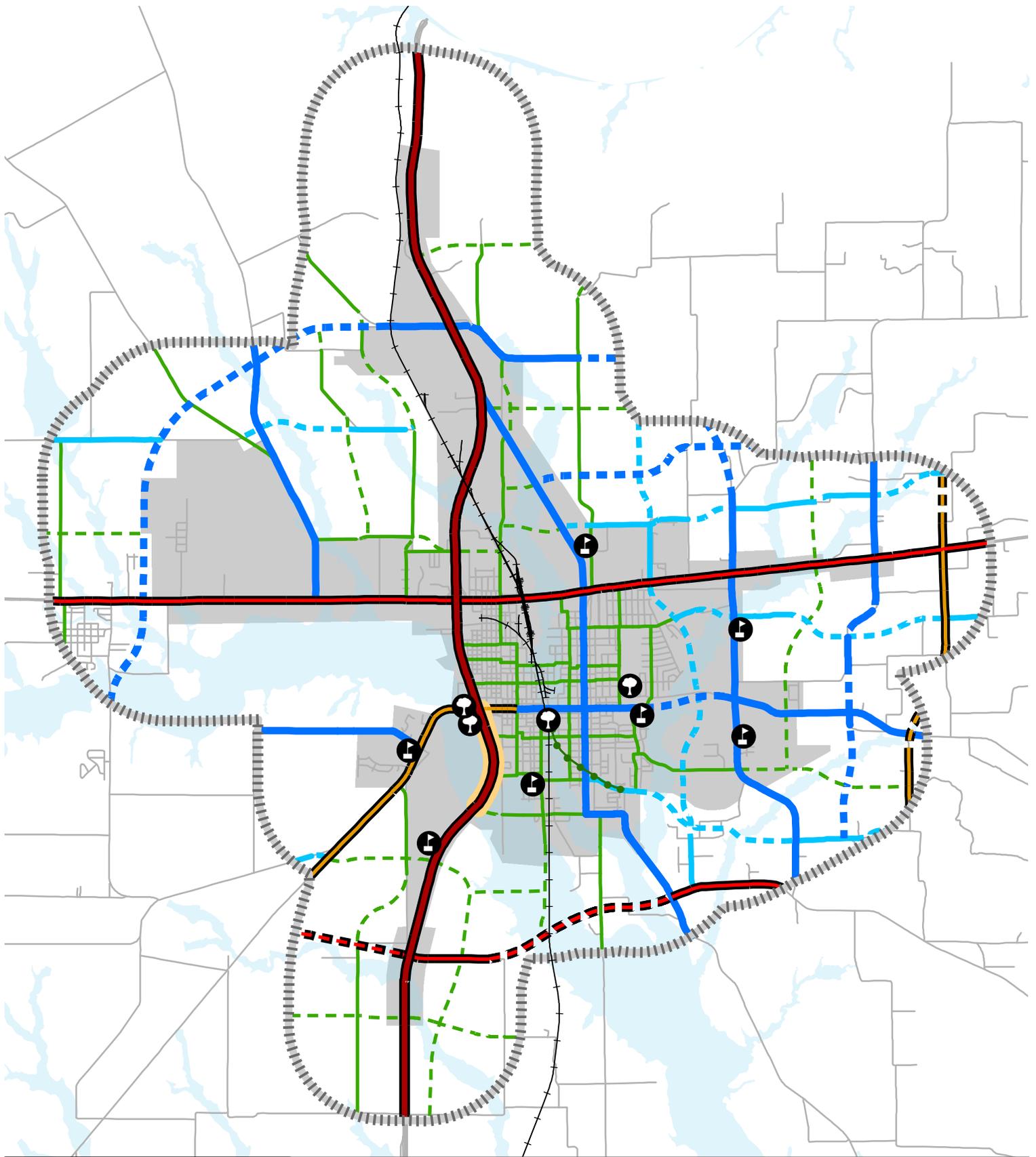
- █ Major Arterial
- █ Minor Arterial
- █ Collector

Status

- █ Existing
- █ Proposed

Other

- █ Roads
- █ Trail
- ⊙ Parks
- ⊙ Schools
- █ City Limits
- █ ETJ
- █ Floodplain
- █ Railroad



2020 THOROUGHFARE PLAN



Thoroughfare Plan

- Freeway/Highway
- Frontage
- Regional Arterial

Principal Arterial

- Major Arterial
- Minor Arterial
- Collector

Status

- Existing
- Proposed

Other

- Roads
- Trail
- Parks
- Schools
- City Limits
- ETJ
- Floodplain
- Railroad